

Australian APT Paper Tiger



NEW production boats



**10 tips from
the Green Flash**

NEW Class Rules

**NEWS from
Clubs
States
Nationals &
Internationals**

September 2016

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Cover photo: Bill Arthur on “Out of Sight” proving that there was some wind at Canberra during last season. Photo by Bill Kerrigan during his OOD duties.

Editorial

Apologies for the delay in producing this issue; it's difficult obtaining the required content. Please consider submitting articles; what regattas have you raced in, how many PTs at your club, what have you done to your boat – whatever.

Thanks to Russell Jolly, Ron Wiggins and Mark Orams for responding to my requests and providing articles for this issue.

Disclaimer

This issue of the Australian Paper Tiger newsletter (APT) has been largely sourced from websites, Facebook posts and archives. It's possible that some data is incorrect.

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Tony Hastings

APT Editor & APTCA Publicity Officer

President's Report

It's been an interesting season and from a personal perspective, a very busy and fun season!

I competed in 7 major PT regattas this season, the Aussie and Kiwi Nationals, the Internationals, the Vic and Tas State Titles and the Audi Showdown and Crown series in Hobart.....oh, and also Hamilton Island Race week with ex PT skippers Dave Barker and Drew Helmore on Daves Nolex 25, in the trailerable division. I probably should feel sailed out after all that, but surprisingly, I'm as keen as ever!

For any of you "club racers" that are a little to very keen, if you haven't already, I recommend making the effort and competing in a National Titles regardless of your level of experience.

For most skippers, you will learn more during one Nationals than you will all season just sailing at your local club. If you then get the opportunity to compete in the Internationals, grab it, being able to represent your country in sailing is quite unique and for me is certainly one of the appealing aspects of racing PTs. If you get to do it NZ, even better, you get to learn even more and more importantly, form new friendships with more like minded people....even if they do speak funny lol

Our Nats were held at Wallaroo SA over Dec/Jan, and although numbers were down on previous years, 4 States were represented and it was a fantastic event in a beautiful location! Thank you to

SA President Russell Jolly especially, for organising the venue and event almost single handed! For those that were considering going but didn't, you should have, it was awesome! ...except for Russell's allocation of 7 sheets of toilet paper, per person each day at the camp site. Needless to say, a low fibre diet was recommended!

The clubs personal did a great job running the event and were very hospitable and generous!

Congratulations to all who participated, but in particular Bryan "Fly'n Bryan" Anderson for taking out his 9th National Title. It came down to the last race with just one point separating 1st & 2nd overall, there was also a variety of conditions during the regatta, so the win was very well deserved!

11 Sippers ventured across the ditch to compete in the Internationals which were hosted by Royal Akarana YC in Auckland NZ. A huge thank you must go out to Swire shipping for their generous support once again getting the boats there and back. It certainly would be financially challenging to hold the event without their support!

Royal Akarana in testing conditions did a wonderful job running the event and were very welcoming and accommodating.

The Kiwi PT association, as usual, did a great job organising the event and billets etc and again as usual, the Kiwis skippers and partners were fun to spend time with.

There was a lot of behind the scenes work done on rule changes this season which have finally been approved and will be officially implemented end of July, although a few of us have already been tinkering!

A huge thank you must go out to all involved in the process.

In fact, it's sometimes taken for granted what it takes to keep an Association such as ours ticking along, so another huge thank you must go out to everyone who contributes in whatever way they do!

The best way though to keep our class vibrant is to just go sailing as much as you can so potential owners of PTs notice us and join in the fun!

I hope to see more of you on the water around the country next season!

Finally a big thank you to Tony for putting together this newsletter!

Happy sailing ☺

- Bruce Rose, President

Paper Tigers – come to the Eden Nationals!

47th Paper Tiger Catamaran National Championship, December 28th to 3rd January 2017

Event website: <http://ausnats2017.papertigercatamaran.org>

It's NSW turn to host the Paper Tiger National Titles, and the Twofold Bay Sailing Club at Eden has been chosen as the venue. This club is on the coast, offering fresh sea-breezes, is open to the sea with ocean swells rolling through, and is as close to the Victorian border as geographically possible.

The club has a beach for rigging up that is protected from wind and waves, extensive parking, large clubhouse and everything needed to host a great series.

While negotiations are still be finalised, it seems likely that the venue may be shared with a small fleet of B14 skiffs. They will sail a different course, to minimise in-water disruptions, and will no doubt add spectacle and fun to the event.

Draft Schedule:

Wednesday Dec 28: 9:00am – 5:30pm Measuring
7:30pm Welcome BBQ

Thursday 29: 10:30am Invitation Race
1:30pm Races 1 & 2

Friday 30: 10:30am Races 3 & 4

Saturday 31: 10:30am Races 5, 6 & 7

Sunday 1/1/2017 Lay Day

Monday 2/1/17: 10:30 Races 8 & 9

Tuesday 3: 10:30am: Races 10 & 11
7:30pm Presentation Dinner

All events at the Twofold Bay Sailing Club, unless otherwise notified. A venue is being sought for the presentation night.

A total of 11 races are expected. Races may be sailed back to back- to -back on each day. The Notice of Race is still being finalised, so some details may change. Check the website (above) for updates.

Prizes

Trophies and/or prizes may be awarded in the following divisions depending on the number of entries in each (or this may be varied at the discretion of the Organising Committee):

- Overall championship
- Junior championship
- B Grade
- C Grade
- Race Winners
- Top Ten

Sponsorship packages

Potential sponsors have been contacted, so far with

little success. Sponsorship is still sought for:

- 1) A main sponsor, who will be included in the event name; “The 2017 (your name here) Paper Tiger Catamaran Australian Championships”.
- 2) Sponsorship of each race: the Invitation Race and each of the 11 heat races.
- 3) Sponsorship of event shirts
- 4) Sponsorship of the presentation evening
- 5) Sponsorship of the welcome night
- 6) Provision of bags, which will include event and promotional materials, given to each entrant.
- 7) Provision of prizes, which may be presented with various prizes or as seen appropriate by the organising committee.

Benefits of sponsorship

All sponsors will be mentioned in media coverage of the event, have advertising in the event handbook, have advertising on the event website, and have marketing material included in the competitor's information pack. Sponsors will also be promoted during speeches at the welcome dinner and presentation night.

Accommodation

Eden is a popular tourist town, with many hotels, motels and caravan parks. Nearby towns of Pambula, Merimbula, Bega and Tathra are within commuting distance of Eden.

The New Year's period is peak tourist season, so prices are up and no discounts are being offered. Local sailors and supporters are being contacted to see if any billets can be arranged. If so, sailors will be contacted directly with an potential accommodation offer.

Weather

Two websites offer information useful for predicting weather at Twofold Bay: Weatherzone, and Willyweather. Both require adjustment; Weatherzone typically fails to add local seabreeze, which comes in around 11, adds 5 knots by 12, 10 knots at 1 and 15 knots from 2pm. So if Weatherzone says 10knots NE and 30+ degrees; expect peaks well over 20!

Willyweather tends to overstate wind, forecasting what might be expected offshore, so subtract 5 – 10knots for the bay.

2015-16 Paper Tiger Nationals at Wallaroo

From VPTCA and other websites:

Wallaroo Sailing Club hosted the 2016 Paper Tigers Catamaran National Championships from December 28 to January 3.

Commodore Margaret Pope said 16 boats from Victoria, Tasmania, New South Wales, Arno Bay and Somerton were tested by the variable conditions.

“The weather was quite calm for the first few days, with winds around five to 12 knots, but for the final days the wind was a south-easterly and kept swinging; at its maximum it would have been about 20 knots,” Pope said.



The series ended with a final dramatic race with the first 6/7 boats crossing the finish line within 3 minutes of each other. Throughout the series all 16 competitors fought it out across a range of wind conditions, stifling heat, minor breakages, including the loss of a tiller at the start of a race and a number of skippers going swimming.



There was a two point swing for Bryan Anderson to win his 9th National title by 1 point over Bruce Rose. Last minute fixes to the trampoline and toe strap paid off. Well Done Bryan!!!



The Wallaroo Sailing Club did a fabulous job running all the races, especially setting the course size and times. This was particularly welcomed by those on shore who were able sit in the shade whilst seeing the starts and finishes of almost all races. Wallaroo was incredibly welcoming to all

sailors and their families. A big thank-you to the club who even sent Wallaroo Sailing Club flags home to all clubs of sailors.

The fun didn't stop onshore. The Sailing Club facilities were brilliant. The air-conditioning and shade were a saviour on the two absolute scorchers. The sandwiches made by the Sailing Club were always welcome at the lunch break and delicious too. Not to mention the incredible feast put on for both the Welcome and Presentation dinners that left everyone full to the brim. The club house was a great spot to bring in the New Year, with nibbles overlooking the incredible firework display.



All initial concerns about the accommodation were completely unfounded! There was more than enough space for all at either the caravan park or cricket ground, a short walk to the Sailing Club. Despite the numerous requests for more toilet paper at the cricket ground it was very secure, accessible and close to the sailing club and all 7 Wallaroo pubs.

The beaches could rival most beaches in Australia, with sparkling blue water, clear enough to see the numerous schools of fish, crabs and seaweed sparsely littered around the ocean floor and lovely soft sand. Nestled under a wheat storage and export facility.



The water was the perfect temperature to frolic in to cool down from the 30 plus temperatures as well as lap it up as a bit of extra exercise when it was too hot for a morning stroll.

The only thing missing was the constant hand swiping and insect repellent spray. But on second thoughts, this was a very welcome treat!

Thank-you to the SA Association for their time and effort organising. Looking forward to the next nationals in Eden NSW!! See you all there!

Results: 46th Paper Tigers Catamaran National Championships

Place	Ties	Sail No	Boat Name	Skipper	Sers Score	Race 11	Race 10	Race 9	Race 8	Race 7	Race 6	Race 5	Race 4	Race 3	Race 2	Race 1
1		2958	Fly'n B'n	A Bryan	A	17.0	2.0	3.0	4.0	[4.0]	[4.0]	1.0	2.0	1.0	1.0	2.0
2		3036	The Apprentice	R Bruce	AV	18.0	[4.5]	4.0	1.0	2.0	1.0	[5.0]	1.0	2.0	3.0	1.0
3		2912	Reco	D Ben	A	26.0	3.0	1.0	[10.0]	1.0	[7.0]	6.0	3.0	3.0	4.0	3.0
4		3104	Firestorm	W Mark	A	28.5	4.5	2.0	5.0	3.0	2.0	[5.0]	4.0	2.0	[5.0]	4.0
5		3039	Mojo	M Ian	AGV	35.0	1.0	5.0	3.0	[7.0]	3.0	3.0	[7.0]	5.0	6.0	4.0
6		3077	Just In Time	A Peter	ASV	53.0	7.0	6.0	[9.0]	6.0	6.0	4.0	[8.0]	7.0	5.0	6.0
7		3050	Boy At Heart	W Mike	A Master	55.0	9.0	8.0	2.0	[9.0]	5.0	7.0	4.0	6.0	7.0	[16.0]
8		3042	Windbreaker	G Trent	B	70.0	6.0	9.0	6.0	8.0	9.0	[13.0]	9.0	8.0	[9.0]	7.0
9		2993	Ere Wiggo Again	W Ron	BSV	78.0	8.0	7.0	11.0	5.0	10.0	10.0	[12.0]	10.0	8.0	9.0
10		3041	Windcruiser	G David	BSV	82.5	[13.0]	12.0	7.0	[13.0]	8.0	8.0	6.0	11.0	11.0	10.0
11		2915	Vendetta	J Russell	B Master	86.0	11.0	[13.0]	8.0	10.0	11.0	9.0	10.0	9.0	10.0	8.0
12		2911	Imagine	D Keith	BSV	104.5	10.0	10.0	[15.0]	12.0	[15.0]	12.0	14.0	13.0	13.0	11.0
13		2994	Mr. Percival	R Neville	C	111.0	12.0	11.0	12.0	17.0F	[17.0S]	11.0	12.0	12.0	12.0	12.0
14		2076	Talisman	E John	C	113.0	[17.0S]	[17.0F]	13.0	11.0	12.0	[17.0S]	11.0	12.0	12.0	12.0
15		2880	Ulinga Warri	D Peter	B	129.0	14.0	14.0	14.0	14.0	13.0	15.0	15.0	[18.0]	15.0	[16.0]
16		2849	Aquaholic	C Perry	CV	135.0	15.0	17.0F	[17.0F]	[17.0F]	14.0	14.0	16.0	16.0	14.0	15.0

2016 Swire Shipping Paper Tiger International Championship

Royal Akarana Yacht Club / Auckland Sailing Club, 24-28 March



Above: all of the sailors at Auckland

From PT2993 ere wiggo again:

Hi all, thought I'd touch base before everything has left my brain.

We sailed out of Royal Akarana yc in Auckland. The weather since we arrived 18 days ago has been warm balmy and humid, sometimes hot as well, except when we arrived back in Auckland it blew an absolute gale for a couple of days and blew itself out.

The club is old and about to be knocked down and 12 million spent on an upgrade.

We launch off hard concrete ramps which has been fine with the help of Anna ,Frida and Pauline (except at low tide very slippery).

Due to the fact they had Opti nationals at the same time, with 250 entries, they forced the RO off his preferred area and in light breezes took 60+mins to get to the start area. Sometimes 60-90 to get home. We had to contend with fast ferries every 10-15

mins creating large wake, container ships, and large power boats and yachts all heading out of Auckland for Easter, it was a washing machine which especially for us heavy guys bought us to a stop. To get going again in the light air and keep up momentum was a challenge.

In addition we had a 2.4 m tide running each day, so I always chose to go right and get away from the tide, get clean air, whereby others went out into it and picked up stronger winds at times, making huge differences by the time you got to the first mark. Sometimes when I got there I was already half a leg behind.

So you'd choose to go out in the tide next time and get no wind, big wake, very frustrating and random. There seemed to be a hand full of sailors that worked it out, I'm not one of them.

The whole Aussie team went poorly apart from Mark who always seemed to get up there even

though he said they were miles faster. Ben at times was there as well but not really anyone else. We only had those two men in the top 10 which in itself is very unusual.

I was quite often within site (even that's unusual) of Bruce, Bryan and other top Kiwis only to lose contact over the last two legs, so I felt I was going ok except when not stalled by the waves.

It's a pity we didn't have our best sailors over here, we needed their thoughts on how the Kiwis have their rigs set up and how they sail different to us, in my opinion it's radical, they have done all the testing and have no doubts it's the reason they are spanking us.

Sail cuts are like the old days huge knuckles at the mast, very full sails, up to two carbon sleeves in masts, drum tight rear uppers (with no sail even up), huge mast rotation and massive amounts of vang tension, all part of their duel systems and very full Dacron and Mylar sails.

Mark Orams who won the series has been training some of the fleet all through winter, he seems like a Sean Keady a shaker and a mover. He's been an Olympic coach, multiple world Laser champ, Americas Cup, around the world and has all the latest testing results from North Sails. He's a goer and you can see the results. The sails are expensive over there.



Above: Mark Orams explains the one-leg karate-chop method of tacking.

We have been thoroughly spoilt by our hosts who happen to be commodore of the club. We have been treated like royalty. The club itself provided the billets and not the association. The welcoming dinner bbq was average but the presentation night meal held at the club was terrific.

We were fortunate to travel around the North Island for a couple of weeks prior to the series. The RO did a terrific job under difficult conditions. Most races were under 10 knots and in fact the best ride of all for me was coming back in on the last

day of racing, flat water hooray and about 12 knots steady.

It's been a big job to get the container here, I believe David Godfrey has sent over 80 emails and he was thanked on presentation night.



Above: David Godfrey capturing some moments

Martin also got us free sunglasses and a cash donation from Mako Eyewear which are great. Check out the website, great glasses.



Above: the Aussie team looking stylish in their Mako sunnies.

We can't forget our main sponsor in Swire Shipping, without them the series would not continue. David Stumbles has done a lot of work with them and insurance for all the boats, a big thank you to him from all the sailors. I think everyone one had a great time. There's a lot to learn this side of the ditch if we believe we are to become competitive again.



Above: variety of sail cuts & materials on display in the rigging area

Results: 2016 Swire Shipping Paper Tiger International Championship

Royal Akarana Yacht Club

Rank	Name	SailNo	Boat name	Club	Age	Team	Race 1	Race 2	Race 3	Race 4	Race 5	Race 6	Race 7	Total	Nett
1st	Mark Orams	3130	Green Flash	Torbay SC	Veteran	Kiwi	(9)	2	1	1	1	3	2	19	10
2nd	Hayden Percy	3073	Aquaholic	Napier SC	Open	Kiwi	2	1	2	3	2	(12)	6	28	16
3rd	Matt Vincent	3106	Miss Hampidjan	Nelson YC	Open	Invitee	1	6	4	5	(8)	1	4	29	21
4th	Mark Wiggins	3104	Firestorm	McCrae YC	Open	Kangaroo	(7)	5	7	7	4	4	3	37	30
5th	Dave Shaw	3084	Feral	Nelson YC	Open	Kiwi	8	4	3	8	(18)	7	1	49	31
6th	Ryan Leatham	3007	The Kraken	Evans Bay Y & MB C	Open	Kiwi	3	13	(21)	12	3	2	10	64	43
7th	Mark Bell	2862	Totally Wired	Torbay Sailing Club	Veteran	Kiwi	4	3	5	(19)	7	14	14	66	47
8th	Mike Hood	3095	The Package	Evans Bay Y & MB C	Open	Kiwi	6	11	11	(15)	10	8	8	69	54
9th	Ben Deed	2912	Reco	McCrae YC	Open	Kangaroo	15	7	13	4	5	11	(38 DNF)	93	55
10th	Scott Hodges	821	Flyer	New Plymouth YC	Open	Kiwi	13	14	6	6	(16)	13	9	77	61
11th	Dylan Taylor	2351	Smugg	New Plymouth YC	Junior	Kiwi	11	(17)	9	2	17	17	13	86	69
12th	Bryan Anderson	2958	Fly'n Bry'n	McCrae YC	Open	Kangaroo	18	(21)	10	21	11	10	5	96	75
13th	Scott Pedersen	3109	Sabre	Napier SC	Open	Kiwi	16	(19)	16	9	12	15	7	94	75

14th	Bruce Rose	3036	Turbo Tone	Lauderdale YC	Veteran	Kangaroo	14	22	(32)	14	6	9	12	109	77
15th	Nathan Percy	2812	Captain Morgan	Napier SC	Open	Tui	10	15	8	(29)	21	18	15	116	87
16th	Ian Marcovitch	3039	Mojo	Mannering Park A SC	Great Grand Master	Kangaroo	17	(28)	17	10	9	19	19	119	91
17th	Stuart Taylor	2989	Whispering Jack	New Plymouth YC	Veteran	Kiwi	(25)	16	19	22	13	6	17	118	93
18th	Peter Robins	3088	Runaway Train	Muritai YC	Veteran	Tui	12	(24)	15	20	14	21	11	117	93
19th	Glenn Syman	2922	Unplugged	Evans Bay Y & MB C	Veteran	Invitee	(24)	20	22	16	15	5	20	122	98
20th	Owen Jenkins	2925	Wind Torque	Torbay SC	Veteran	Tui	19	18	18	13	(28)	20	22	138	110
21st	Mark Hatch	3075	Bad Boy	New Plymouth YC	Veteran	Tui	5	26	(38 DNF)	11	23	27	21	151	113
22nd	Denis Leatham	2988	Learning to Fly	New Plymouth YC	Grand Master	Tui	23	12	14	(28)	26	22	16	141	113
23rd	Ian MacLennan	2820	Love Over Gold	Torbay SC	Legend	Tui	(28)	9	23	23	22	16	28	149	121
24th	Nick Gardiner	822	Abby Cat	Queen Charlotte YC	Junior	Tui	20	10	12	26	24	(35)	31	158	123
25th	Jamie Hatch	2634	Roar Speed	New Plymouth YC	Open	Tui	21	25	20	(33)	19	23	18	159	126
26th	Les Gallagher	3093	Arriba	Howick SC	Great Grand Master	Tui	26	29	(30)	18	20	25	24	172	142
27th	Mark Suckling	2971	SilverFox	Torbay SC	Veteran	Invitee	(30)	27	24	25	29	28	23	186	156
28th	Lynley Manning	2800	One Fish - Two Fish	Evans Bay Y & MB C	Open	Tui	29	33	26	17	31	(34)	27	197	163
29th	Bill Caunce	2972	Mustang Sally	Tauranga	Grand Master	Invitee	22	23	25	24	35	(38 DNS)	38 DNS	205	167
30th	Carl Syman	2861	Psyclone	Clarks Beach YC	Open	Invitee	(35)	30	28	31	25	30	25	204	169
31st	Ron Wiggins	2993	Ere Wiggo Again	McCrae YC	Great Grand Master	Kangaroo	(31)	31	27	30	30	29	30	208	177
32nd	Jason Hegert	3377	Synergy	Ballarat YC	Open	Wombat	33	(38 DNF)	38 DNS	27	27	24	34	221	183
33rd	Keith Deed	2911	Imagine	McCrae YC	Great Grand Master	Kangaroo	(34)	34	33	34	33	26	26	220	186
34th	Glen Sowry	3010	Animal Print Pants	Royal Akarana YC	Veteran	Invitee	27	8	(38 DNF)	38 DNS	38 DNS	38 DNS	38 DNS	225	187
35th	Martin Gregory	2577	Phantom Tiger II	Wagga Wagga SC	Open	Kangaroo	(36)	32	31	32	34	33	33	231	195
36th	Russell Jolly	828	Rumpus Cat	Somerton YC	Legend	Kangaroo	32	(36)	29	35	36	32	32	232	196
37th	Peter Darling	3077	Just in Time	Arno Bay YC	Open	Kangaroo	(37)	35	34	36	32	31	29	234	197

Top Ten Tips for Paper Tiger Racing

by Mark Orams PT 3130 "The Green Flash"

I came to the Paper Tiger from a monohull background and I had never sailed a multihull before. During my first season I found at times I was fast, but at other times I would be slow. I also made many mistakes as my mind was set in "monohull mode". After a few months this became quite frustrating and so I decided to start a log-book and note down key learnings from all training and sessions and racing. These notes are still helpful to me today even though I am more experienced in the boat. Hopefully they have some value for other ex monohull sailors transitioning to the PT but also to long-time multi-hull sailors as well.

1. **Stay smooth and in the groove**
 - Keep the helm steady – place your tiller extension on your thigh when hiking to help with this.
 - Work the mainsheet to keep the windward hull stable and just flying above the water surface.
 - Sheeting for stable flight is more important than maximum hiking.
2. **Tacks are costly**
 - Sail the PT through an arc to tack – don't try to "flick" from one tack to the other – you will stall and park the boat.
 - A good tack costs 12 seconds and a bad one 20 seconds plus so remember the advantage gained by tacking must be greater than 12 seconds.
3. **Don't underlay the top mark**
 - See point 2 above.
 - Even being tight on a layline can cost you as you lose so much speed trying to squeeze up to get around the mark.
 - If you overlay, you can go 'bow-down' and gain 10-20% more boat speed and roll people on the layline below you.
4. **Don't get stationary less than 1 minute from the start gun**
 - Keep flow over your rudders or you're a sitting duck. Once you stall it's tough to get going forward again.
5. **Sail arcs around marks**
 - The PT is a wide and short boat and so does not like being turned sharply. Sail semi-circular courses around marks to keep the speed on.
6. **Drive it low and go, go, go**
 - Pinching is not good, the foils are too thin. The modern PT foils are designed to generate lift with speed, so let them do that. Put the bow down and "send it".
7. **Pressure is king**
 - Small windshifts of less than 5 degrees are not nearly as important as pressure (wind-strength). A small increase in pressure will allow you to sail much higher upwind and much lower downwind.
 - When looking upwind – look for pressure and position yourself to get it.
8. **Mistakes are costly**
 - Because a multi-hull is travelling so fast, any mistake (eg. a bad tack, underlaying a mark, capsizing, having to return to a start for an individual recall) costs you heaps of ground. So, take low risk options, keep it simple.
9. **Get down the back of the bus when reaching in breeze**
 - PTs have a tendency to "sniff" when reaching in over 18 knots – so get your arse behind the back beam when going downwind in strong breeze.
 - Make sure you have your centreboards up, vang off, inner forestay on and hike back (not out) to keep the nose out. This will allow you to keep the power on and sail faster without nose-diving.
10. **Work with training partners to make incremental improvements**
 - When you are trying new ideas (equipment, settings, technique) it is impossible to tell if they are improvements unless you have another Paper Tiger to test against. Having a training partner or partners allows you to work together to make incremental improvements.
 - It's also really helpful to have a view/opinion from off your boat – you often miss things and the perspective of another experienced Paper Tiger sailor can be really helpful.
 - A training partner is also an advantage when you are at regattas, you can test your set up before the start, chat about how you see the wind and current and encourage and support each other.

Go fast, sail smart, have fun!

Interview with the Green Flash

Following is an email exchange, in which Mark Orams was kind enough to answer questions put to him.

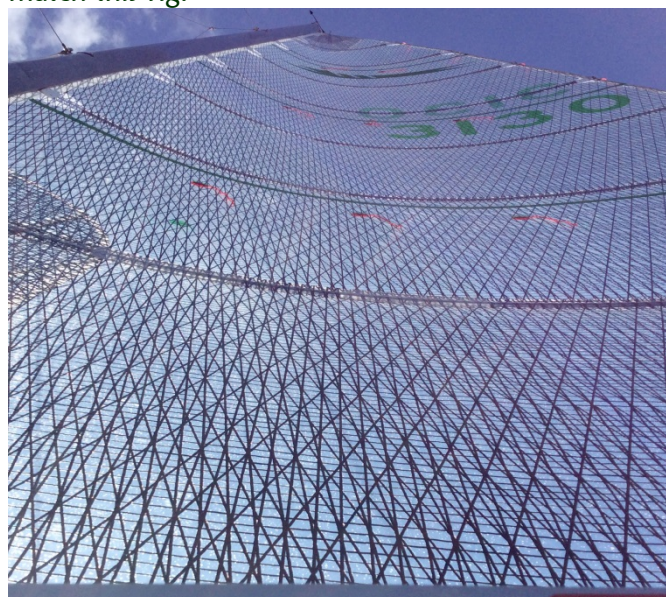
Question: In tip 4, you say "don't get stationary within 1 minute from the start gun". Surely this means with 1 minute to go, you must be some distance back from the start line and have your path obstructed by a wall of boats in front of you. How do you get through the wall?

Alternatively, how do you approach the start line?

Answer: If you are in a large fleet and with a relatively short line then you need to protect your ³lane² heading into the line. This means you can get ³compressed² on approach whereby you need to slow or stop so you don't go over the line early, or just as bad, being stationary and in "irons" at the gun. So my suggestion is to keep as much distance as you can from the line while still protecting your lane. If you are forced to stop or slow so much you no longer have control (i.e. No flow over the rudders), you need to know how to get out of irons quickly and get going again as soon as you can. Having larger rudders helps with this process (especially if they have a wide span at the water surface so you can get good "bite" at low speeds. It's certainly worth practicing.

Question: It seems that after being thoroughly beaten at the 2016 Internationals, most of the Australians are fitting mast stiffeners and boom vang's, as they think this was the key equipment that gave Kiwis superior speed. Do you agree?

Answer: As with all aspects in yacht racing - a speed advantage is never one thing - it's always a combination of many things. Mast stiffeners are a waste of time if you do not have the right sail to match this rig.



Above: the right sail, made by North Sails

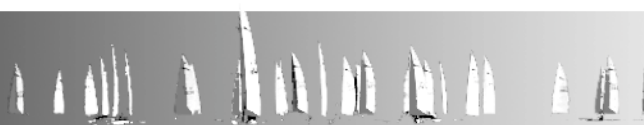
Similarly, having a vang independent from your mast rotation is not an answer in itself, but if it is used in combination with a mast and sail set-up and technique that suits this set up then it works really well. In developing new approaches and seeking speed gains it is important to understand what the change you are going to make is expected to do. Then it is critical to test out the change one thing at a time and over a range of conditions so you can get a good sense of whether it is a gain or not and what the trade-offs are (there is rarely a gain in all conditions and points of sail). For example, a stiffer mast requires a flatter sail – so wacking a stiffener up your rig and using a sail developed for a mast without one will not be a step forward.



My sails are very flat by comparison to most (but my body weight is 78kg so I still need power) – so I generate power through a stiff rig. The independent vang allows me to "vang sheet" when windy and also carry a lot of mast rotation - so more mast bend when I need it and a straight leech for acceleration and drive.

Question: When there is light winds and chop, a Paper Tiger can "hobby horse" and come to a stop, with the nose bouncing up and down and the sail flapping. Do you have a sail trim and steering strategy in this conditions?

Answer: As with most PT sailors, when it is light



and bouncy and you start “chopping wood” in the PT I tend to sheet wider and sail lower angles. I also concentrate hard on steering around waves and lifting the windward hull higher over big sets if I can. As a principle I try to keep my body weight low and compact just behind the front beam (the balance point for most PTs). Keeping weight low is a big deal - I am surprised that so many PT sailors work so hard to get their boats down to weight and then they put half a kg of water in a bottle stuck on the front of their mast! Doesn't seem smart to me.

Question: In Australia, we are seeing a continuous drop in sailor's numbers. The Paper Tiger fleet is reducing as sailors move to other classes, or retire, and we are not recruiting new people. Although the Paper Tiger is a fantastic training boat, few or no clubs utilise them for training youths. For example, Yachting Australia announced that the (non-existent) Nacra 15 "will be used domestically for selecting multihull athletes to the Australian Youth Sailing Team (AYST) for the Youth World Sailing Championships and for nominating athletes to the Australian Olympic Committee for the Youth Olympic Games from 2016 onwards."

What can we do to prosper as a fleet?

Answer: I think strategically inviting some of the hot young youth sailors to contest PT regattas can be a big help - and then publicise that. We had 2012 Olympic Gold Medalist (and now 2016 Silver medalist) Polly Powrie sail a PT in the first part of our nationals - our class president Mark

Hatch loaned her a boat. This was great publicity for the class and Polly was more than happy for us to “leverage” off her positive reviews of the PT. Teenagers want to sail against other teenagers and not a class that is perceived to be for old men - so having up and coming hot teenage sailors as invitees do guest appearances in the class can be a good starting point.

The main two constraints we see here in NZ are finding high quality second hand boats for sale (it annoys me that so many good PTs have been sitting in garages for years and the owners do not sail them - but refuse to sell them). The other issue here is storage. Because they take a while to rig and take up a lot of room in a garage, clubs which have space for PTs to be stored with their rigs up are a real bonus for the class.

Question: Are there any other comments you would care to include?

Answer: The PT is a great boat with really good people in it who are open to share ideas and help one another. The culture of the class is awesome - it is critical we keep this. Participation rates in classes come and go and then come back again, so I would not get to worried about being in a low period right now. Remember that you Ozzies smashed us Kiwis in the internationals for over a decade, it's gone our way lately, but it will swing back the other way at some stage. As long as we keep having fun and enjoying fair, friendly competition it'll work out well. Cheers!



PAPER TIGER CATAMARAN INTERNATIONAL ASSOCIATION

World-wide governing body for the Paper Tiger Catamaran Class

Rule Change Proposals 2015

Final Results of Voting

The period for voting on the eight rule change proposals closed on 18th December 2015. A total of 69 voting papers were returned from five associations. The distribution of those returns, and the total eligible memberships for the associations, are shown in the table (right):

Association	Voting Papers Sent Out	Voting Papers Returned	% Members Voting
New Zealand	89	28	31%
New South Wales	21	12	57%
South Australia	17	15	88%
Tasmania	16	2	13%
Victoria	31	12	39%
TOTALS	174	69	40%

The voting result for each proposed rule change is shown below. The Paper Tiger Class Rules state “Amendments to these Rules shall be by at least two thirds majority of all ballot papers returned by financial Paper Tiger Catamaran Association members”. As such, any proposal receiving an approval of 66.66% or greater is deemed to be successful. All proposals achieved this.

Note: Where total of Yes and No doesn't equal voting papers returned, this accounts for abstains.

Percentage is based on number of Yes votes from total forms returned.

The new rules come into place on 1st July 2016.

Proposal	Topic	Yes	No	Result
001	Loose Foot Sails	55	13	79.7%
003	Mast Rotation Control	54	14	78.3%
004	Preventing Hydrofoiling	58	11	84.1%
006	Gunwales	60	8	87.0%
008	Hulls Identification	66	2	95.7%
009	Hulls Dispensation	66	1	95.7%
010	Measurement Certificates	64	3	92.8%
011	Reefing Points	56	11	81.2%

Following are the Class Rules, as new in effect with the amendments made. Apologies that the diagrams have been shrunk for sake of space.

Paper Tiger Catamaran International Association, By-Law 1, Class Rules

1-GENERAL

The PAPER TIGER catamaran is a one-design class. Boats shall conform to the general requirements and outlines shown on the plans except as modified by these Class Rules.

In accordance with the World Sailing Equipment Rules of Sailing, these Class Rules fall under the definition of clause C.2.3 “Open Class Rules” which state: “Class rules where anything not specifically prohibited by the class rules is permitted”. Interpretation or clarification of the Class Rules shall be requested in writing from the International Measurers.

A measurement certificate is to be issued for all boats measured and a record kept by the National Measurer.

Boats that do not comply with the Class Rules shall be liable for disqualification from class events, in accordance with the World Sailing Racing Rules of Sailing.

For the purpose of measurement, should the class rules be silent on a measurement matter (including measurement procedure), the World Sailing Equipment Rules of Sailing procedures shall apply.

NOTE: Details of fittings and method of construction on the plans are typical only. Alternate fittings (eg mast top pulley, mast base, mast spanner, traveller track, etc.) may be used providing they are not prohibited elsewhere in these Class Rules.

2-HULLS

1. Hulls may be made of any material.
2. Decks shall be straight in the athwartships

direction. All hull panels to be flat in section profile allowing for natural twist in hull material between frame sections.

3. Hull length shall not exceed 4267mm excluding gunwales measured parallel with the designed waterline. This length shall be measured by drawing a straight line which transits the top of the main beam and a point 406mm above the keel at the transom.

4. Overall beam shall not exceed 2133mm excluding gunwales.

5. Gunwales (Rubbing Strakes or Sheerguard)

a) Gunwales may be made of any resilient material.

b) Gunwales shall be mounted externally.

c) Maximum dimensions: Width: 25mm, Height: 30mm.

d) Minimum dimensions: Width: 13mm, Height: 18mm.

e) Maximum width between main and rear beams: 40mm.

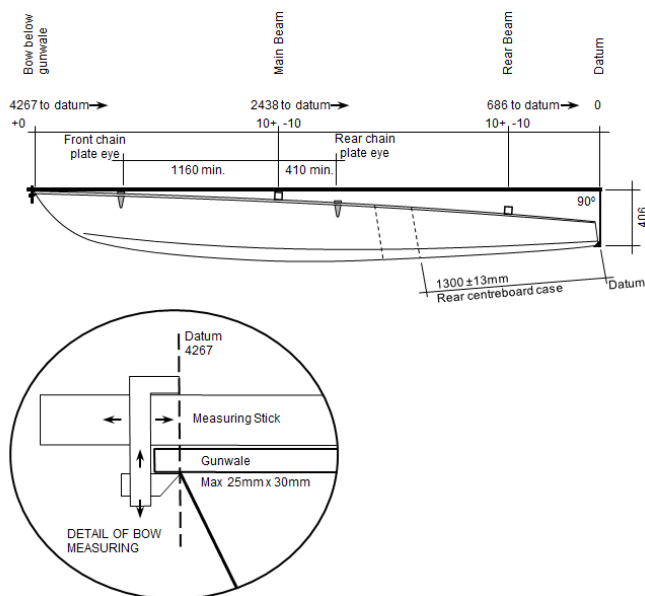
f) Width measurements are to be taken from the topsides of the hull. The top edge is to be flat and in the same plane as the deck (may be checked by a straight edge).

g) Minimum radius at the bow: 14mm in plan view.

h) The underside of the gunwale may be flat, convex, concave or tapered.

6. Hull measurements shall be in accordance with the plans and shall have a maximum tolerance of + or - 6mm on total width or depth. Maximum tolerance shall be + or - 12mm on bow profile. Maximum radius on keel and chine shall be 6mm except forward of station 2 on the chines.

7. Measuring templates will be used at stations 2, 6, 10 and 14 and on bow profile.
8. The registration number, with a minimum size of 20mm, shall be indelibly marked inside the hulls, viewable through an inspection port.
9. Hulls which do not measure may be given dispensation under the conditions set out in BY-LAW 7. The issue requiring dispensation and confirmation is to be noted on the reverse of the measurement certificate and placed on the class register.



3-BEAMS AND TRAMPOLINE

1. Main and rear beams to be 50mm x 50mm + or - 1.0mm x 3.2mm + or - 0.3mm aluminium alloy square tube.
2. All beams must be mounted flush on the deck.
3. "L" brackets may be used to attach the beams at the inner attachment points of each hull only. Such "L" brackets shall have maximum dimensions of 50mm x 50mm, and be constructed of aluminium alloy of a minimum of 4mm and maximum of 6mm in thickness.
4. Rear beam to be 686mm and main beam to be 2438mm each + or - 12mm to centrelines from transom at keel, measured parallel to waterline at deck level. Beam ends shall be angled at 45 degrees + or - 4 degrees.
5. A trampoline support of aluminium section shall be securely fixed along the centreline of the boat.
6. Trampoline material and attachment method to be optional.
7. Jumper strap shall be made from either stainless steel or aluminium alloy flat section and be within the following size limits.

	Minimum	Maximum
Stainless steel		
Width	25mm	50mm
Thick	3mm	4mm
Aluminium alloy		
Width	30mm	50mm
Thick	4mm	6mm

In addition to the above, aluminium alloy jumper straps shall have a minimum cross section of 160 square mm.

All sharp edges of jumper strap to be removed or protected (eg with electrical tape).

No part of the jumper strap shall extend beyond the inner gunwale.

8. Straight traveller tracks may be used instead of the hawse shown on the plans.

Traveller length, both solid and hawse, shall not exceed the length of the top surface of the rear beam. The hawse may be made from wire or alternative material.

9. Use of a single aluminium alloy extrusion that incorporates the rear beam, the traveller track, and a trampoline attachment track is permitted as long as it meets all other associated class rules.

4-WEIGHT

1. Minimum hull weight to be 50.0kg with all fixed fittings weighed in dry conditions excluding rudderstocks, mainsheet, but including inspection port covers and fixed compasses. Boats shall be EITHER weighed to one decimal place, OR the weight rounded to one decimal place.
2. For the purposes of weighing, each boat shall be freely suspended in a sling from an approved scale or spring balance.
3. Weighing of each end separately is not allowed.
4. Allowance shall be made for the weight of the slings.
5. Any weight required to bring a boat up to the minimum weight shall be fixed inside the ends of the beams as follows:
 - (a) Boats weighing more than 47.5kg - one half of weight in each end of main beam.
 - (b) Boats weighing 47.5kg or less - one quarter of weight in each end of main and rear beams.

5-CHAINPLATES

1. Chainplates must be positioned on the outer side of each hull.
2. No hole in the rear chainplate shall be within 410mm of the centreline of the main beam.
3. No hole in the front chainplate shall be within 1160mm of the centreline of the main beam.

6-CENTRECASSES

1. Centrecases: rear of slot to be 1390mm + or -

15mm along the keel from outside of transom.

2. Maximum dimensions of centreboard slot measured at deck level shall be 25mm x 310mm and at keel level 25mm x 305mm. Recesses for centreboard stops are permitted providing they do not exceed 40mm in depth from deck level.
3. Centreboard must not be fitted with any device or means for angling centreboard to port or starboard.

7-CENTREBOARDS AND RUDDERS

1. No restriction on centreboards and rudders shall apply other than that the centreboards shall not deviate from the vertical plane of the centreboard below the waterline and the maximum centreboard dimensions as in rule 6(2) above.
2. The centreplane of each hull, its centreboard case and its rudderstocks (in the fore and aft position) shall coincide.
3. The maximum athwartships dimension of the rudder or rudderstock shall be 80mm within 100mm of the waterline projected from the transom.
4. The maximum distance from the transom to the centreline of the rudder pintle points shall be 70mm.

8-SPARS

1. Mast and boom shall be of a parallel aluminium alloy extrusion of minimum size 50mm x 64mm, maximum size 62mm x 80mm and minimum weight of 0.95kg/m. Tapering of mast extrusion is prohibited but internal stiffening is allowed.
2. Boltrope track must be continuous except for below the gooseneck and for boltrope entries. Separate boltrope tracks shall be considered part of the extrusion for measurement purposes.
3. Length of mast with top and bottom fittings not to exceed 6780mm from top of main beam to top of mast fitting.
4. Boom shall not exceed 2290mm in length including gooseneck plug.
5. Hounds attached to mast at 2590mm and 5180mm + or - 40mm measured from top of beam to shackle pin position. Shackle pin position shall be no greater than 25mm from the mast extrusion surface.
6. Mast must be sealed.
7. Boom block hangers - dimensions on plans are advisory only. There is no restriction on exact location, but block locations shall not be alterable while racing.
8. Downhaul control fittings shall be mounted on the mast and/or the spanner.
9. The boom vang shall be connected to one point

only on the boom and shall be attached to either a saddle or a spanner, attached at or near the base of the mast.

10. Lever fittings to give greater boom vang purchase are permitted but must comply with items 9 above.
11. Outhaul control fittings shall be located on or within the boom only.
12. The tails of control lines may be lead to positions of convenience and the slack taken up as long as the primary control load is taken by fittings positioned in accordance with the relevant class rule.

9-RIGGING

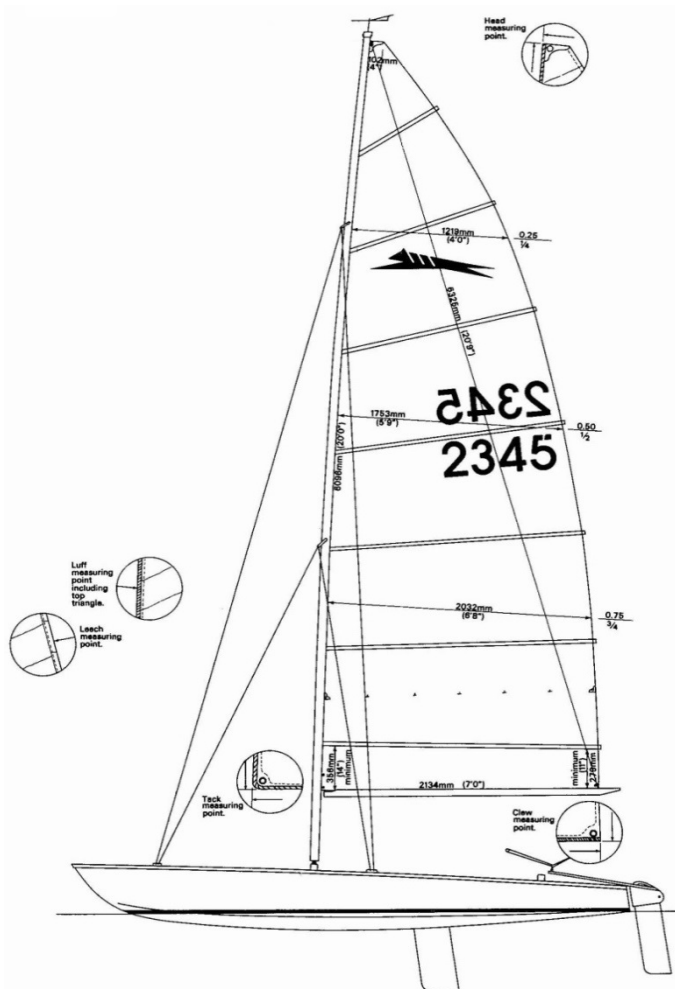
1. The rigging is to be in accordance with the plans except configuration of the lower forestays is optional. In addition, shackles and adjustable devices may be used to attach the stays/shrouds to the chainplates. Only the lower forestays may be adjusted while racing. Rigging dimensions quoted are advisory.

10-SAIL

1. To be measured in accordance with the World Sailing Equipment Rules of Sailing, to meet the following restrictions and table of dimensions as represented by the drawing.
Note: Sail measurement dimensions and restrictions outlined herein supersede those on sheet five of the plans when in conflict
2. Leech measurement is taken in a straight line from head measuring point to clew measuring point.
3. 7 full length battens only.
4. There is no restriction on batten material.
5. The bottom batten position is restricted. The dimensions restricting the location are measured to the inside edge of the stitching that forms the bottom of the batten pocket.
6. Leech profile shall be a fair curve.
7. Leech cord can be internal, external or non-existent.
8. Leech cords shall be adjustable on the sail or boom only, and control line tails not lead to another place.
9. Sail Numbers shall be positioned in accordance with the World Sailing Equipment Rules of Sailing.
10. Reef points may be fitted if desirable.
11. Any sail used in a loose foot configuration is to have a maximum foot round as per table below. The foot of the sail is to be an arc and the foot round is the maximum deviation of this arc from a straight line drawn between the tack point and the clew point.

12. The sail measurement dimensions are as per the following table and the drawing.

Sail measurement position	Dimensions	
	Maximum	Minimum
Luff	6,096mm	
Leech	6,325mm	
Top width	125mm	
Upper leech point	762mm	
Upper width	570mm	
Three quarter width	1,080mm	
Half width	1,695mm	
Quarter width	2,030mm	
Foot	2,134mm	
Foot round (as defined in point 11 above)	130mm	
Tack point to bottom batten @ luff		356mm
Clew point to bottom batten @ Leech		270mm



11-RACING

1. The craft may be raced single-handed or with a crew but the number must remain constant

throughout any sanctioned event.

2. Boats shall comply with the safety regulations of the Member National Authority of their own country.

3. At sanctioned events, only in the event of major damage can a mast, a sail, a hull assembly, a single hull, a main beam or a rear beam be changed and such change shall have the approval of the sailing committee.

4. Only fully paid members of the Paper Tiger Catamaran Association shall be eligible to race in sanctioned events.

12-THE FOLLOWING ARE PROHIBITED

1. Any device for adjustment of the bend in the mast while racing except for lower forestay, mainsheet system, downhaul and boom vang.

2. Trapeze or sliding seat.

3. Pocket luff sails.

4. Curved travellers

5. Fairing of streamlining of beams or masts.

6. Downhaul adjustment using drum winches, geared winches or levers.

7. Any device for altering batten tensions, excluding leech cords, while racing.

8. Electronic or mechanical devices for measuring speed of wind or yacht.

13-AMENDMENTS

1. Amendments to these Rules shall be by at least two thirds majority of all ballot papers returned by financial Paper Tiger Catamaran Association members.

2. A ballot paper may be handed out or sent by mail, fax or electronic media to all members. Only one vote per member shall be returned in the allowed time.

3. A ballot paper must contain at least two arguments for and against the proposed change.

4. A maximum of sixty days from date of posting the ballot paper must be allowed for return mail.

5. All ballot papers must first be reviewed and approved by all national measurers before being sent to members.

6. Any approved rule change shall apply from the first day of the next July or such date as may be determined by the Paper Tiger Catamaran International Association.

7. This By-Law comes into force on the 1st of November 1999.

New Paper Tigers

From a Facebook post by Larry Fay:

The 50th anniversary of Fay Family built Paper Tigers is coming up and my son Marc and I have started working on new PT moulds. This marks the 3rd generation of Fays building PTs and these will be our best hulls yet.

The current moulds are nearly 25 years old, so we'll be updating the new tooling to a modern design from this century that will in turn produce more accurate and even higher quality hulls.

All hulls whether Fibreglass or Full Carbon Fibre will be built underweight. Our last PT hulls weighed just 15.2kg gel coated, leaving plenty of weight spare to build the platform. The standard hulls will include a white gel coat finish, 6 full size foam sandwich bulkheads, extended and more comfortable bum rails and will also will be stronger and stiffer. We also plan to keep a stock set available for immediate purchase once the moulds are complete however, lead time on custom ordered hulls such as an inbuilt trampoline track in the decks, could be as short as a month. We're hoping that producing new PTs will boost the lack of quality second hand boats for sale which may attract new sailors into the sport.

At the moment we would like to gauge interest for new boats which will determine how quickly the moulds are completed.

For more information on pricing of hulls, or package deals please private message myself or visit our website www.ausfg.com.au for more contact details.

Thanks, Larry and Marc Fay

Below: the PT plug on which moulds can be made

We've just finished work building a new PT plug. We've made multiple changes to the old hulls such as the Gunnel and Bum-rail to a more attractive shape that also deflects water easier with less drag. The hull itself has be re-faired, has straighter lines, a sharper transom and overall much better finish that reflects the technology being used in 2016.

These will be foam sandwich vacuumed bagged and joined vertically in the moulds like current A classes. 6 foam sandwich Bulkheads, no timber used for zero rot, Gloss white Gelcoat finish with additional options like non-skid decks and in-build deck tramp tracks. Full carbon with Kevlar reinforced hulls are also available. It's common for previous hulls to last well over 20 years and remain very competitive.

We're now taking orders for new hulls for the upcoming season. However there is limited time so only a few boats can be produced before this season starts.

I've attached pictures of the new plug. It would be great if you could please create a post on the NZ Paper Tigers FB page.

Thank you.

Regards,

Larry Fay

Australian Fibreglass Composites pty.ltd
2/16 Amberley Crescent, Dandenong South
Victoria, 3175

Ph: +61 3 9792
1227

www.ausfg.com.au
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NZ Female Jig

Also seen on Facebook:

Some pics of the weekend labours. By Stu Taylor on the Dan Leech easy build system.

Things are now starting to look like a boat. You will need to view files in number sequence to get an indication of the process flow. Will send more as she takes shape

Stu



News from Paper Tiger State Associations

Tasmania

Well done to Rob Cracknell on the overall win. Bruce Rose 2nd Sean Keady 3rd. 1st B Grade Mike Church 2nd Scott Wilkie 3rd Richard McMinn. Stand out of the regatta Danica Taylor. Many thanks to Sandy Bay Sailing Club and everyone that turned up to support the class. Overall a hard fought and highly competitive championship. Crackers said “thanks to all the guys who organised it and all you other guys for turning up making it a

great weekend of sailing. Well done Danica; great sailing after such a short time on the boat. Thanks to all the other sailors for giving me such a hard time we can only improve with that sort of rivalry. “I must say I was blessed with my favourite conditions. Thanks for all the hard work of the TPTCA.”

Paper Tiger Tasmanian State Championships 2016

Sandy Bay Sailing Club

Rank	Fleet	Boat	Class	SailNo	Club	HelmName	CrewName	Rating	Race 1	Race 2	Race 3	Race 4	Race 5	Race 6	Total	Nett
1st	Paper Tiger	Macattack	Paper Tiger	3100		Rob Cracknell			1.0	1.0	(2.0)	2.0	2.0	1.0	9.0	7.0
2nd	Paper Tiger	The Apprentice	Paper Tiger	3036		Bruce Rose			(15.0 DNF)	2.0	1.0	1.0	1.0	3.0	23.0	8.0
3rd	Paper Tiger	Sssmokin Billy	Paper Tiger	2997		Sean Keady			4.0	(7.0)	3.0	3.0	3.0	2.0	22.0	15.0
4th	Paper Tiger	RomperStomper	Paper Tiger	2809		Steve Price			5.0	3.0	4.0	(7.0)	4.0	4.0	27.0	20.0
5th	Paper Tiger	3	Paper Tiger	2927		Mick Boyle			2.0	5.0	(15.0 DNC)	5.0	5.0	5.0	37.0	22.0
6th	Paper Tiger	Wasabi	Paper Tiger	2207		Danica Taylor			3.0	6.0	(15.0 DNC)	4.0	11.0	9.0	48.0	33.0
7th	Paper Tiger	Go With The Flow	Paper Tiger	2679		Patrick Amos			6.0	8.0	6.0	(9.0)	6.0	7.0	42.0	33.0
8th	Paper Tiger	Barbadian Czar	Paper Tiger	2773		Mike Church			8.0	4.0	9.0	6.0	8.0	(10.0)	45.0	35.0
9th	Paper Tiger	Tenacious	Paper Tiger	2727		Scott Wilkie			9.0	(12.0)	8.0	8.0	7.0	6.0	50.0	38.0
10th	Paper Tiger	Que Sera	Paper Tiger	3066		Richard McMinn			7.0	9.0	5.0	(10.0)	10.0	8.0	49.0	39.0
11th	Paper Tiger	Gulf Buggy	Paper Tiger	3020		Brendan Amos			10.0	(11.0)	10.0	11.0	9.0	11.0	62.0	51.0
12th	Paper Tiger	Aquaholic	Paper Tiger	2849		Perry Caulfield			11.0	10.0	7.0	13.0	(15.0 DNC)	15.0 DNC	71.0	56.0
13th	Paper Tiger	Monsoon	Paper Tiger	2702		Martin Sullivan			12.0	13.0	(15.0 DNC)	12.0	12.0	12.0	76.0	61.0
14th	Paper Tiger	Runningamuck	Paper Tiger	2799		Murray Willis			(15.0 DNC)	15.0 DNC	15.0 DNC	15.0 DNC	15.0 DNC	15.0 DNC	90.0	75.0

Victoria

McCrae News

Wrong way has been sold to Lindsay Ellis who has been chasing boats for a while. He is currently making a new trailer and we hope to see him on the water sooner than later and hopefully attend one of our training sessions, one was held recently held at Mt Martha by the VPTCA. We hope he'll join McCrae YC. Neil Wilson who sold Wrong way has upgraded to Apache and has made significant gains up the McCrae fleet . (A boat Luke Stout tuned for Sarah). The fleet has been smaller than usual since xmas with 5 boats in the container competing in NZ.

Evan Rutherford a new McCrae member has also been chasing boats for a while. He sailed PTs in New Zealand. As this goes to print it looks he's bought Michael Thorns boat from Mt Martha. Ross Tebbutt has bought a boat for this season and is sailing at the club. He sailed a PT probably 25 yrs ago , he also attended the training day and is pleased as his performance improves. So all is looking good at the club for next season, the tigers are the biggest individual cat class at McCrae. Pt 2993 'ere wiggo again

New South Wales

The Williams dominate the State Titles – again

Congratulations to the father-and-son team of Garry and Greg Williams, who dominated the NSW State Titles, held at Koonawarra Bay on the Australia Day long weekend. The pair had consistently fast boat speed, and excellent decision making in light shifty conditions.

Greg Williams was extremely fast, almost flawless in his tactical decisions, and won with a perfect

score of 4 wins. Garry Williams gave him a good contest, finishing a close second in three of the races.

The only other race winner was Rohan Nicol, who struggled with a gooseneck and bent mast track that hampered him and caused him to retire from a race.

Results:

Place	Sail No.	Boat Name	Skipper	Club	Grade	Category	1	2	3	4	5	Points
1	3086	Shadow Boxing	Greg Williams	Wagga Wagga	A		1	1	1	4	1	4
2	3085	Characin IV	Garry Williams	Wagga Wagga	A	Veteran	2	2	6	2	3	9
3	2901	Tigerdelic	Tony Hastings	Wallagoot Lake	A		3	6	3	11	4	16
4	2826	Pussyfoot	Rohan Nicol	Wagga Wagga	A		7	7	DNF	1	2	17
5	3040	Second Wind	Wayne Eager	Koonawarra Bay	A		5	5	4	3	6	17
6	3076	Rapture	David Stumbles	Koonawarra Bay	A		6	DNF	2	7	5	20
7	2967	Johnny B Goode	Jon Pinkerton	Koonawarra Bay	A	Veteran	4	3	5	10	8	20
8	3018	People Eater	Neil Waterman	Koonawarra Bay	A		8	4	9	9	9	30
9	3016	Wet Dreams	Steve Levi	Concord Ryde	A	Veteran	11	8	7	5	12	31
10	2888	Hot & Spicy	Peter Breaden	Tanilba Bay	A	Veteran	9	10	DNF	6	7	32
11	3023	Zena	Bob Nicholls	Pigeon Bay	I	Veteran	10	9	8	8	11	35
12	2457	Bonny Jane	Paul Scotland	Concord Ryde	B	Veteran	13	11	11	13	10	45
13	2577	Phantom Tiger II	Martin Gregory	Wagga Wagga	B		12	12	10	12	13	46

Tigerdelic takes 2015 - 2016 traveller trophy

The State Regatta Pointscore series (traveller's trophy) was comprised of 5 events, with 4 to count. These included:

- Batemans Bay Anzac Regatta
- Choice of Wagga Wagga or Jervis Bay Regattas
- ACT Multihull Champs (YMCA Canberra)
- Mannering Park 14' Cat Regatta
- State Championships at Koonawarra Bay.

Unfortunately Ian Marcovitch was delayed returning from the Wallaroo Nationals, and missed the State Titles. This left me as the only contestant to compete in 4 of the events, with an unbeatable points gap over the fleet. I was pleased to represent Paper Tigers at these regattas and score top results against other classes.

- Tony Hastings

Results:

Place	Sail #	Boat Name	Skipper	Club	B Bay	JB / Wagga	Mannering	Canberra	KoBSC	Points
1	2901	Tigerdelic	Tony Hastings	Wallagoot Lake	1	1	DNC	2	3	7
2	3039	Mojo	Ian Marcovitch	Mannering Park	DNC	2	1	DNC	DNC	35
3	2826	Pussyfoot	Rohan Nicol	Wagga Wagga	DNC	DNC	DNC	1	4	37
4	2888	Hot & Spicy	Peter Breaden	Tanilba Bay	DNC	DNC	2	DNC	10	44
5	3016	Wet Dreams	Steve Levi	Concord Ryde	DNC	3	DNC	DNC	9	44
6	2577	Phantom Tiger II	Martin Gregory	Wagga Wagga	DNC	2	DNC	DNC	12	46
7	2457	Bonny Jane	Paul Scotland	Concord Ryde	DNC	4	DNC	DNC	11	47
8	2625	S.K.I	Johan Zoutendyk	Wagga Wagga	DNC	1	DNC	DNC	DNC	49
9	3086	Shadow Boxing	Greg Williams	Wagga Wagga	DNC	DNC	DNC	DNC	1	49
10	3085	Characin IV	Garry Williams	Wagga Wagga	DNC	DNC	DNC	DNC	2	50
11	3098	Out Of Sight	Bill Arthur	Wagga Wagga	DNC	DNC	DNC	3	DNC	51
12	3040	Second Wind	Wayne Eager	Koonawarra Bay	DNC	DNC	DNC	DNC	5	53
13	3076	Rapture	David Stumbles	Koonawarra Bay	DNC	DNC	DNC	DNC	6	54
14	2967	Johnny B Goode	Jon Pinkerton	Koonawarra Bay	DNC	DNC	DNC	DNC	7	55
15	3018	People Eater	Neil Waterman	Koonawarra Bay	DNC	DNC	DNC	DNC	8	56

The State Regatta Pointscore Series for 16/17

At the AGM, it was discussed to only have 3 regattas including CRSC and the State Championships, with the idea that this may encourage participation.

This was later revised to have 4 regattas. Rather than being within one season, the series goes from State Titles to State Titles so that the trophy can be presented there.

This season's regattas include:

- Concord Ryde Sailing Club – Marathon - March 2016 (past)
- Mannering Park Sailing Club – 14ft Catamaran Regatta 8-9th October 2016
- YMCA Canberra – Multihull Regatta – 22-23 October 2016
- The **2017 State Championships** at Koonawarra Bay Sailing Club again on 28-29th January 2017.

The venue for the States was initially planned to be Canberra, but it was decided that KoBSC would be likely to have better attendance and better weather.

Club News

Canberra

Bill Arthur

The YMCA SC runs three competitions through the sailing season. The Alternate series which is very short course racing with up to 4 races sailed in the afternoon. This series is run when there are regattas that Club members may want to attend. Enabling them to attend without affecting their performances in the other two series. Then there is the Point Score series which is effectively the Handicap series with up to 24 races sailed in the season. Two races are sailed each day in this series. The Major event is the Championship series which is held over 4 Saturdays in the season with two heats sailed each day.

In the Alternate series Rohan Nicol sailing "Pussyfoot" finished 5th. (and last). In the Point Score series Rohan finished 4th with Bill Arthur on Out of Sight"6th. (no, he wasn't last). In the Main event, the Club Championship Rohan took out first place ahead of Peter Ward's Hobie 14 Turbo. Bill Arthur was third in a fleet of 7 mixed catamarans.

Unfortunately Nick Weight the third Tiger owner in the ACT was unable to make an appearance due to the pressures of Uni and part time work. I understand he has moved south to a Victorian Uni..

Round 1: Concord Ryde

The marathon race hosted by the Concord-Ryde Sailing Club was both round 1 of the Paper Tiger State Pointscore, and a heat of the Maricat National Titles.

The local Paper Tigers of Steve Levi, Paul Scotland and John Taylor were joined by Ian Marcovitch and Neil Waterman.

Steve reported that it was a good day, and that they followed Paul down the Harbour and around most of the course, with Ian close behind (possibly to see which way to go!). They went around Cockatoo Island, some other islands, to Birkenhead Point, back uptowards the clubhouse, then into Hen and Chicken Bay, around a buoy in some other bay... finally there was a long run down to the finish. That was when Ian took the lead, with Steve also overtaking Paul before the line. Neil finished 4th.

The next Concord Ryde marathon will be held 3rd December.

The Season at the YMCA SC was difficult with more than our fair share of very light wind days. Two of the scheduled Championship days were abandoned and had to be resailed later in the season.

Rohan has the ply to start building a replacement for his ever reliable "Pussyfoot". Progress is slow, however in Rohan's words, "It HAS to be ready for Eden!!!!!!"

This year's ACT Cat Champs is listed on the Programme as 22-23 October.

I forgot to mention that Rohan also won the Cat division of the Chief Ministers Regatta on Australia Day. This event is a one race regatta with no entry fee run by the CYC with good prize money donated by the ACT Govt. This year conditions were again light with 4.3m cats dominating the results. Rohan won well from the Hobie 14 Turbo sailed by YMCA SC member Peter Wood third was the Mari Pointed Reply (Rob Anderson). I would have been 4th had my entry been processed properly and placed in the Cat section. I was recorded as finishing last in the fast monohull section.

Other Cats that took part included relatively modern A classes, Hobie 16's,18's and Taipans.

Concord Ryde

Steve Levi

Our club committee decided that in our area there are a lot of units being built – 26 story units. They might be interested in sailing but have nowhere to store their boat. If we created a space, we might get more membership.

A new extension almost doubles size of clubhouse, and is built over the water on piers. It was funded by a Government grant plus club funds, mostly on materials – piles in the ground and stack of hardwood.

Volunteers built the deck, put it all together -I was down there today installing security cameras.

An 80year old builder member was the person behind building it.

Some of the hardwood beams – it took five guys to lift one end of it. We lifted an end at a time, used a massive drill bit, put a stainless bolt through, it was phenomenal.

1st race 3rd September.

Wallagoot Lake Boat Club

Tony Hastings

I'm proud to be the Club's Multihull Champion again, for the 5th year in a row. PT2901 "Tigerdelic" is significantly faster than the local Windrush and Maricats, and gives a Stingray cat a run for its money around the course.

Top-right: Tigerdelic leads the Stingray at Wallagoot.



Every second weekend is a handicap-start race. Rather than start on 35 minutes and try to complete the short course in 20, I sailed Laser Radial, 49er and NS14, to claim 2nd in that series.

My NS14 "Superfly" is sailed with a young crew, Luca, who is learning fast. We won a handicap prize in our first regatta, at Twofold Bay in February, then scored 2nd place at the Wallagoot Regatta in March, and claimed the win at the Batemans Bay in April.

We plan to alternate skipper & crew roles next season, which gives me a shot at being the first ever to win multihull and monohull championships, and Luca a chance to win the handicap series. Unfortunately it also means no Paper Tigers will race at Wallagoot this season, though I plan to race Tigerdelic in local regattas.

In the photo **below**, I demonstrated how my Paper Tiger skills transfer to the NS14 by nosediving down a wave during the Batemans Bay Anzac Regatta. We still managed to win that race, as the other entrants in our division had even more trouble than we did.





Regattas and Titles Series

	Queensland	Tasmania	South Australia	Victoria	New South Wales
October					1-2 Jervis Bay & Wagga Regattas 8-9 Mannering Park 14' cat regatta 22-23 ACT Multihulls
November				12-13 Cairn Curran Regatta 26-27 McCrae Cat Champs	26-27 Kembla Klasic
December				11 Training Day Mt Martha	
28 Dec – 3 Jan, 47 th Paper Tiger National Championship, Twofold bay, Eden, NSW					
January				23-26 Yarrowonga Australia Day	28-29 State Champs Koonawarra Bay
February				11 Training Day Somers YC	11-12 Twofold Bay Regatta
March				4-5 State Champs, Frankston	11-12 Wallagoot Lake
April					22-23 Batemans Bay Anzac

International Championship	National Championship	State Championship	State Pointscore Series	General Regatta
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(No internationals in 2017)

Presumably there will be Tassie and SA State Titles, contact the association for further details.



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